

# NORTH SCOTT CORRIDOR PLAN

CITY OF BELTON, MISSOURI

CONFLUENCE  
DECEMBER 11, 2014

# ACKNOWLEDGEMENTS

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# 1 INTRODUCTION

## BACKGROUND

Belton, Missouri is a rapidly growing municipality located in northwest Cass County with a current population of approximately 25,000 residents. The community is located about 17 miles south of Downtown Kansas City along the Interstate-49 corridor. Growth in Cass County and the proximity to highway corridor has provided strong retail demand in the area. In addition to the mixture of local businesses and civic uses in Downtown Belton, larger retail commercial development was initially centered along the Highway 58 corridor/interchange. Recent improvements to the 163rd Street interchange and Markey Parkway has also attracted retail commercial development anchored by Academy Sports + Outdoor and Menard's.

The northernmost Interstate-49 interchange serving Belton is 155th Street, which is slated for improvement by MoDOT in 2015-2016. This interchange provides access to the northern portion of the North Scott Corridor, located just west of the interchange. This interchange also provides access to the CenterPoint-Kansas City Southern Intermodal Center, a 370-acre facility developed on the grounds of the former Richards-Gebaur Air Force Base. This facility provides opportunities for strategically located distribution centers, with the ability to ship goods to 80 percent of the U.S. population within 2 days by truck. Also located approximately 2-miles north of this interchange is the new 1,500,000sf National Nuclear Security Administration facility, projected to employ approximately 2,500 workers.

The City of Belton began an initial corridor study effort led by a student intern in 2013, which identified opportunities for enhancing the appearance of the corridor and creating additional amenities and attractions to serve the surrounding community,

including Downtown Belton located near the southern end of the corridor. As part of this study, the existing (non-functioning) Belton Grandview and Kansas City Railroad alignment that runs parallel to the North Scott Corridor was also being considered for use, either permanently or on an interim basis, as a community trail connection.

## STUDY AREA

The North Scott Corridor study area is approximately 2 miles in length, from 155th Street on the north to Highway 58 on the south. It is bounded on the west by the railroad alignment and on the east by the Interstate-49 Highway corridor. The extension of Markey Parkway will provide an east-west connector through the middle of the study area, and is anticipated to be completed in 2015 to provide a vehicular connection to the 163rd Street interchange. This connection will improve access to and through the central portions of the North Scott Corridor, and is anticipated to create new development opportunities as a result.



FIGURE 1.0 EXISTING SITE PHOTOS ALONG NORTH SCOTT



FIGURE 1.1 EXISTING ENTRANCE AT 155TH STREET AND NORTH SCOTT

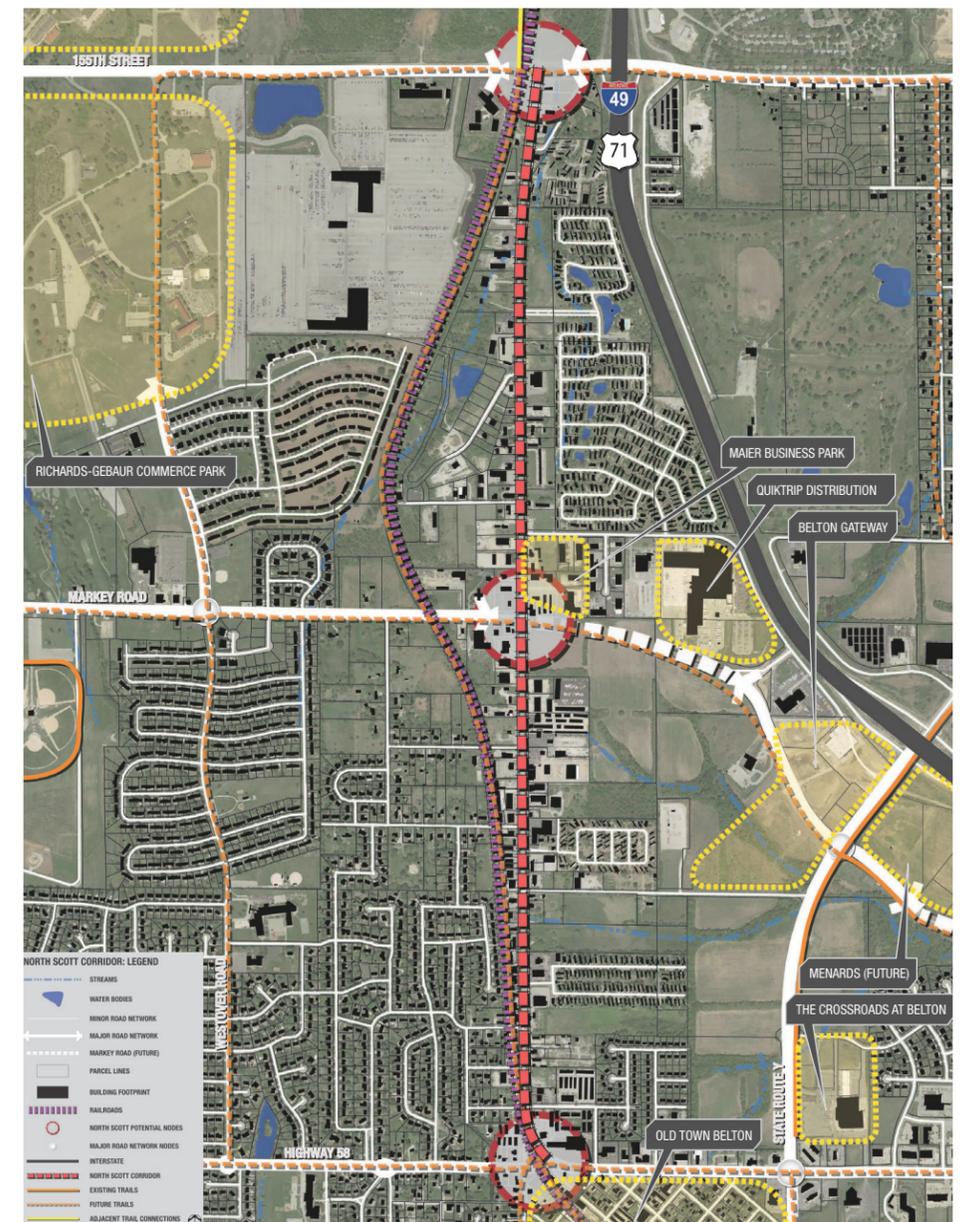


FIGURE 1.2 EXISTING CONDITIONS MAP



FIGURE 1.3 EXISTING SITE PHOTOS ALONG NORTH SCOTT

**EXISTING CONDITIONS**

The North Scott Corridor currently contains a wide array of commercial, industrial, and residential uses. While the businesses along the corridor have historically served the day to day needs of the Belton community, the needs of the community have evolved over time – and much of the day to day retail shopping has moved to newer commercial centers to the east of the study area.

There are several businesses and restaurants that continue to prosper as community destinations, but they share the corridor with numerous buildings that exhibit signs of aging and deterioration. There are also several aging residential manufactured home communities visible from the corridor. This mixture can create a challenging development climate, and can affect the long-term economic health of the corridor. The proximity of the railroad tracks creates many smaller irregular-shaped parcels of land along the west side of the corridor, with properties getting incrementally smaller on the southern end. The largest parcels of developed and undeveloped land primarily exist along the east side of the corridor.

The street is four lanes wide north of Markey Parkway, and three lanes wide south of Markey Parkway. Existing sidewalks and streetlights are located on both sides of the corridor, with numerous driveways providing access to properties throughout the corridor.



**STUDY GOALS**

Through this study effort, the City of Belton is defining a vision to guide future revitalization of the corridor. As part of an interactive process, the planning team collaborated with members of the City's Advisory Committee to discuss their initial thoughts and opinions regarding the role this corridor should play in supporting Belton's continued growth. As a result of these discussions, the following series of goals were established for this corridor study:

- 1 CREATE AN ECONOMIC DEVELOPMENT STRATEGY FOR CORRIDOR GROWTH THAT ENCOURAGES A THRIVING AND DIVERSE MIXTURE OF COMMERCIAL ENTERPRISES THAT TAKES ADVANTAGE OF BELTON'S UNIQUE LOCATION.
- 2 STRENGTHEN EXISTING USES AND PROPERTIES THROUGH STRATEGIC REVITALIZATION ACTIVITIES AND PUBLIC-PRIVATE PARTNERSHIP OPPORTUNITIES THROUGHOUT THE CORRIDOR.
- 3 ENHANCE THE CORRIDOR'S IMAGE BY IMPROVING ITS PHYSICAL APPEARANCE, AMENITIES, AND CONNECTIVITY WHILE ESTABLISHING A UNIQUE IDENTITY AND SENSE OF PLACE.

The North Scott Corridor serves as the northern gateway into Belton. With proper planning and attention, its potential can be realized for attracting high-quality growth and revitalization to capitalize on the area's economic development synergies and to better serve the growing needs of the surrounding community. By engaging the community to establish a future vision for the corridor, this planning effort is intended to begin a multi-phased effort to strategically position the area for long-term success.

# 2 PLANNING PROCESS

In July of 2014, the City of Belton selected Confluence to facilitate a community-driven planning process to engage stakeholders and the surrounding community in creating a vision to guide the corridor's future. The recommendations outlined in this plan were derived from this open and transparent process.

The planning team coordinated a series of three meetings with an identified Advisory Committee comprised of business and property owners along the North Scott Corridor, area neighborhood leaders, local experts, public officials, and Belton citizens. Each of these meetings provided opportunities for the Advisory Committee to better understand the issues being explored as part of the planning process, to ask questions and obtain additional information, and to provide their input and direction for shaping the proposed plan recommendations.

In addition to the Advisory Committee meetings, the overall schedule involved two public meetings. The first of these public meetings was a Visioning Workshop to provide input and preferences for the future "look and feel" of the corridor. The second public meeting involved an open house format to review a draft version of the corridor plan document, and to provide additional information regarding the planning process, the options explored, and the recommendations highlighted in this plan. The final plan is anticipated to be presented to the City Council in January of 2015.

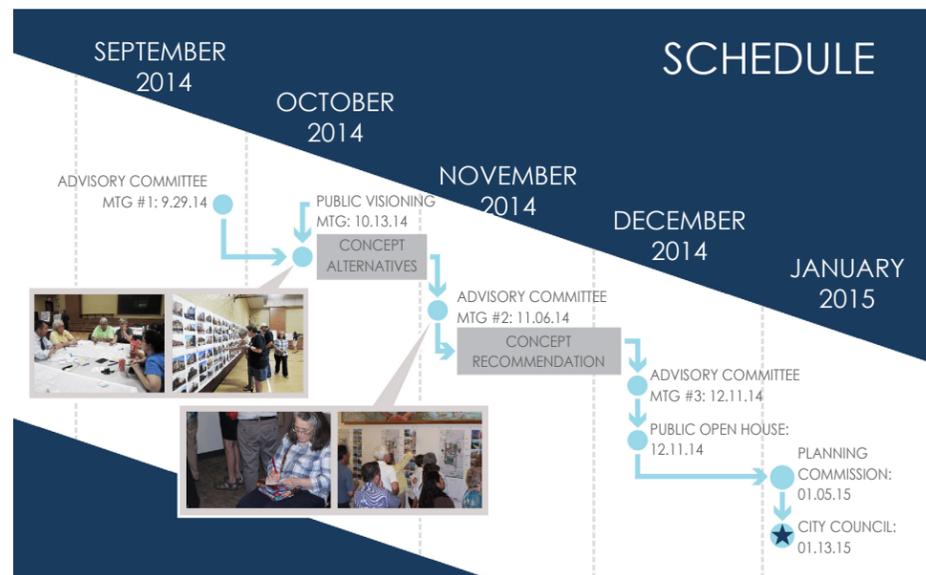


FIGURE 2.0 ADVISORY COMMITTEE INVOLVEMENT

## ADVISORY COMMITTEE INVOLVEMENT

The City of Belton identified and selected members of the community to serve on an Advisory Committee for the duration of this study effort. The intent of this committee was to serve as a resource for the planning team to identify issues along the corridor, to evaluate concepts and options for addressing these issues, and to provide direction and input in the formation of the overall corridor planning effort.

The committee included representatives with varied interests along the corridor, including business owners, property owners, neighborhood residents, elected and appointed officials, and commercial brokers/agents. Each of these members was encouraged to represent the anticipated views and opinions of the respective constituent groups they represent, and to work collaboratively with the planning team to explore alternatives and to find consensus on preferred solutions that can move this plan into the implementation stage in the near term.

Through a coordinated series of meetings starting with a kick-off meeting on September 29, 2014, this committee participated in hands-on design and planning activities to define the existing character and function of the corridor today, and to envision what how they'd like the corridor to be transformed in the future. They also assisted in the development of the three primary goals for this study, and provided valuable input regarding the community's involvement in this planning process. Members of the committee were invited to actively participate in the Visioning Workshop, and to subsequently review and assist in shaping the proposed recommendations for revitalizing the corridor.

## COMMUNITY INVOLVEMENT

It was important to engage the public in this planning process to ensure their issues and observations are identified and addressed as part of the plan recommendations. This involvement included two public meetings, and outreach via the City's website providing opportunities for the public to complete a questionnaire early in the process – providing valuable information and input upon which the planning team could base their initial concepts and ideas.

The initial public meeting consisted of a Visioning Workshop, held on October 13, 2014. The planning team, in conjunction with City staff, facilitated a 2-hour meeting that provided the citizens of Belton an opportunity to express their thoughts and concerns for the future of the North Scott Corridor. Meeting participants received a presentation about the goals of the study and then were asked to participate in three different exercises: providing their preferences for the area's future visual character; providing input on a brief questionnaire; and then developing initial plan concepts and ideas for improving the corridor (see Figure 2.1).

For the first exercise, the planning team provided over 300 precedent images displayed on the walls of the meeting room. These images were organized into three distinct categories: Development Character, Amenities and Connectivity, and Streetscape Character. Each participant was provided five colored dots or "votes" in each category, and were instructed to vote for their favorite images to reflect their vision for the corridor. Participants were also provided five red dots to place on images they felt were not appropriate for the corridor. This exercise is extremely valuable to provide the planning team with a collection of images that summarize the community's initial direction for enhancing and improving the corridor. This collection of images can be further expanded and utilized in subsequent phases of the planning effort to refine and identify specific planning opportunities and recommended design solutions.

In addition to questionnaires being completed by workshop attendees and member of the Advisory Committee, the planning team received an additional 68 survey results from posting it on the City's website. This reinforced the importance of the North Scott Corridor and the role it plays in Belton's future, and is a positive reflection of Belton's citizens interest and involvement in their community.

The last exercise of the Visioning Workshop included organizing participants into small groups to focus on documenting their ideas and concepts for improving the corridor. Each group was provided a large map of the corridor and a kit of markers, colored post-it notes, and tracing paper to capture their thoughts. Participants also were able to link their preferred images to their respective concepts, using the number of each image to illustrate where they wanted to see specific improvements made within the study area.

Once groups were finished, one representative from each group was asked to provide a brief report of their initial concept for the North Scott Corridor. These preliminary ideas served as the basis for the development of the planning team's preliminary land use concepts.



FIGURE 2.1 PLANNING PROCESS EXERCISES



**NORTH SCOTT AVENUE CORRIDOR PLAN**  
PUBLIC VISIONING MEETING #1  
October 13, 2014

**QUESTIONS:**

- List 5 words that describe the corridor today:
- List 5 words that describe how you'd like the corridor to be in the future:
- What types of uses, activities or features are missing along the corridor?
- If you left Belton, Missouri for 10 years and then returned, what portions of the North Scott corridor would you hope remained unchanged?
- What portions would you hope have been improved?
- What do you consider to be the "front door(s)" for Belton?

FIGURE 2.2 COMMUNITY INVOLVEMENT





GROUP PLANNING EXERCISE

During the visioning workshop, attendees were organized into five different groups to prepare their initial ideas and concepts for improving the North Scott Corridor. The groups focused primarily on land use concepts and selected their preferred location for a community anchor - which took different forms in each concept.

Each of these five concepts is briefly summarized and illustrated on this page, including the main ideas that differentiated the recommendations of each group.

These were used by the planning team to form the basis for the next stage of the planning process - refining and combing these various ideas into a refined collection of land use plan alternatives.



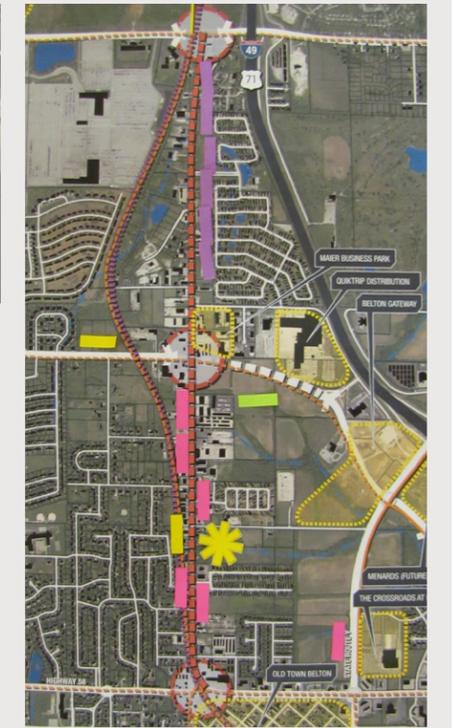
**GROUP 2 MAIN IDEAS:**

- INDUSTRIAL CORE
- COMMUNITY ANCHOR ALONG CORRIDOR
- RETAIL SPREADING ALONG MARKEY
- LARGER RETAIL ENTRANCE AT 155TH
- MULTIFAMILY TO SUPPORT INDUSTRIAL JOBS



**GROUP 4 MAIN IDEAS:**

- INDUSTRIAL ENTIRELY NORTH OF MARKEY TO 155TH
- COMMUNITY ANCHOR UTILIZING NATURAL FEATURES
- RETAIL CORE AND SMALL NODES AT MAJOR INTERSECTIONS



**GROUP 1 MAIN IDEAS:**

- COMMUNITY ANCHOR ALONG RAIL
- EXECUTIVE OFFICE SOUTH OF MARKEY
- RETAIL NODES AT 155TH, MARKEY, AND TURNER ALONG NORTH SCOTT



**GROUP 3 MAIN IDEAS:**

- NORTH GATEWAY COMMUNITY ANCHOR
- NORTH EXECUTIVE OFFICE
- RETAIL AT MARKEY NODE
- MULTIFAMILY SOUTH OF MARKEY
- WALKING TRAIL ALONG RAIL



**GROUP 5 MAIN IDEAS:**

- HEAVY INVESTMENT IN RETAIL AT 155TH AND MARKEY PARKWAY
- STRONG RECREATIONAL CONNECTION ALONG TRAIL
- STRIP OF INDUSTRIAL NORTH OF MARKEY



# 3 ANALYSIS + IDEAS

## COMMERCIAL CORRIDOR CONCEPT

The Commercial Corridor concept (shown in Figure 3.0) was heavily focused on industrial development along North Scott. Retail development concentrated at 155th Street and Markey Parkway. Office development would utilize the interstate visibility, and multifamily was placed along Highway 58 from North Scott to Y Highway. The community anchor was placed in a central location along the corridor, near Markey Parkway. This feature could become a trail head and/or a park amenity located along a converted rail/trail corridor.

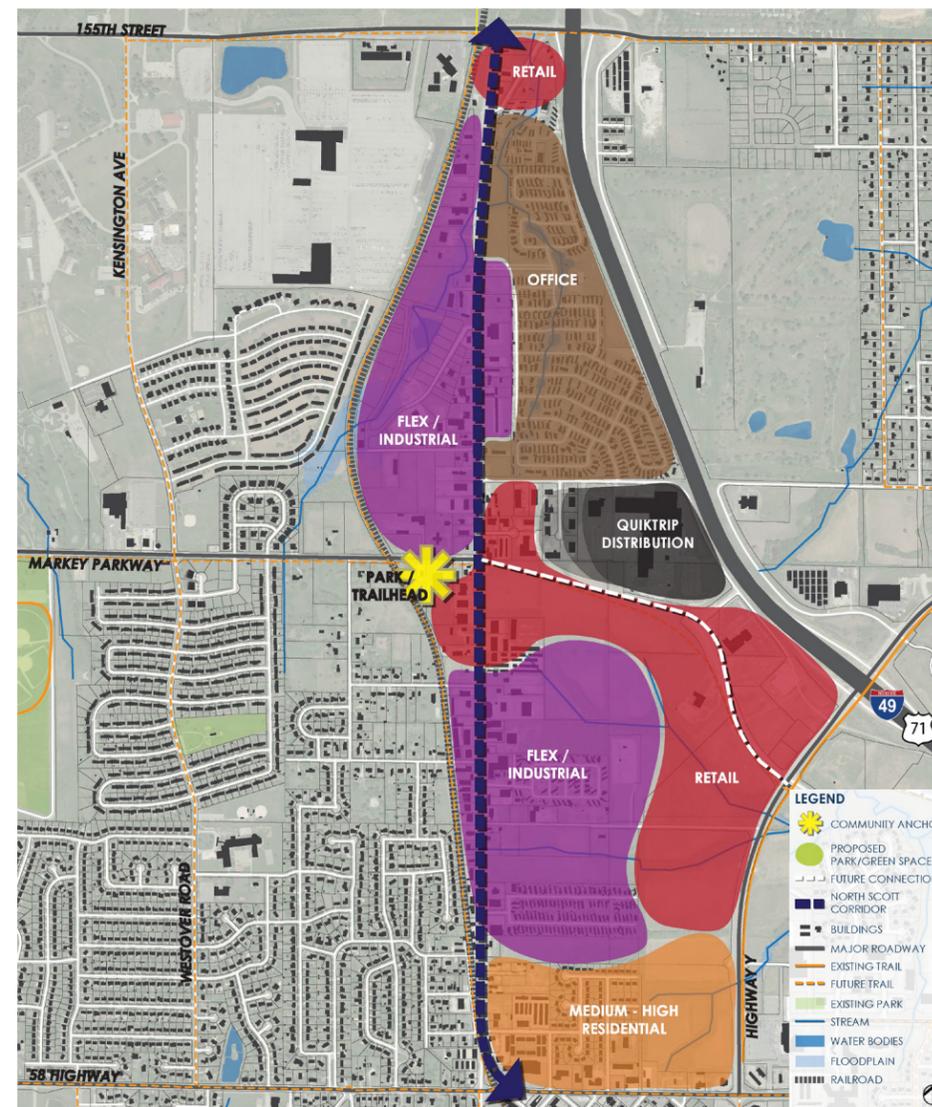


FIGURE 3.0 COMMERCIAL CORRIDOR CONCEPT

## COMMUNITY CORRIDOR CONCEPT

The Community Corridor Concept (shown in Figure 3.1) concentrates flex/industrial land uses north of Markey Parkway. There is a larger retail focus at the Markey Parkway and 155th Street intersections along North Scott, and smaller retail nodes towards the south part of the corridor. Green space would replace shallow commercial lots along the west side of North Scott at the south end of the corridor and include a community anchor feature as part of this park amenity.

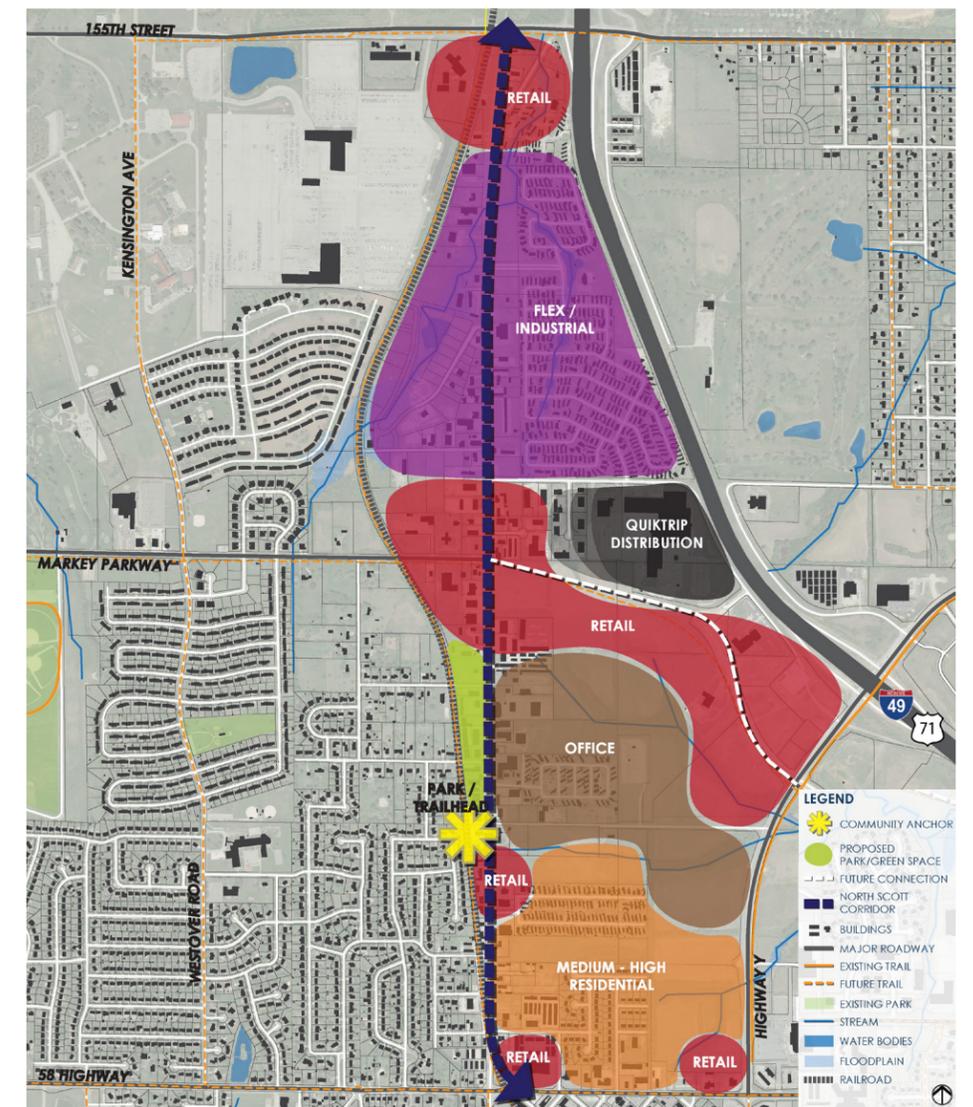


FIGURE 3.1 COMMUNITY CORRIDOR CONCEPT

### DATA ANALYSIS

Once the activities and questionnaires conducted during the visioning workshop was completed, the planning team began to analyze and process the information and initial ideas that were developed.

Utilizing this information in conjunction with the team's own observation of the study area conditions and opportunities, a series of three refined land use concepts and roadway configuration alternatives were developed. Each of these built on major ideas and themes generated during the visioning workshop.

### LAND USE CONCEPT ALTERNATIVES

On November 6, 2014, the planning team met with Advisory Committee members to share, discuss, and evaluate three refined land use concept alternatives. A summary and an illustration of each of these concepts is provided as follows:

MIXED USE CORRIDOR

The Mixed Use Corridor Concept (shown in Figure 3.2) illustrates the community anchor as a gateway feature at the 155th street intersection. The concept shows the most blending of land use options, keeping some key retail nodes that are important to the corridor today. Green space is shown replacing the shallow retail lots along the corridor's west side, south of Markey Parkway. A mixture of office and flex/industrial land uses is shown north of Markey Parkway. This concept also shows retail adjacent to the entire Highway 58 stretch from North Scott to Y Highway.

LAND USE PLAN ANALYSIS + PREFERENCES

Once the planning team presented the three land use concept alternatives to the Advisory Committee, members were given an opportunity to provide their comments and suggestions. At the end of the discussion about these alternatives, they selected their preferred land use concept, including specific elements or features from other concepts they felt would improve their preferred concept (see images in Figure 3.3).

The Commercial Corridor Concept was the preferred land use alternate, and won by a wide margin. Discussion centered on minimizing the amount of office shown north of Markey Parkway, and exploring potential office and mixed use development along

the east side of Interstate-49. Over half of the participants preferred the community anchor location to be at the corner of 155th and North Scott, stating that development of a gateway feature here would draw more traffic into Belton through the North Scott Corridor. The Advisory Committee wanted to retain existing key businesses along the corridor. Retail along 58 Highway was preferred, and a desire to reduce the amount of medium density residential in this area was expressed.

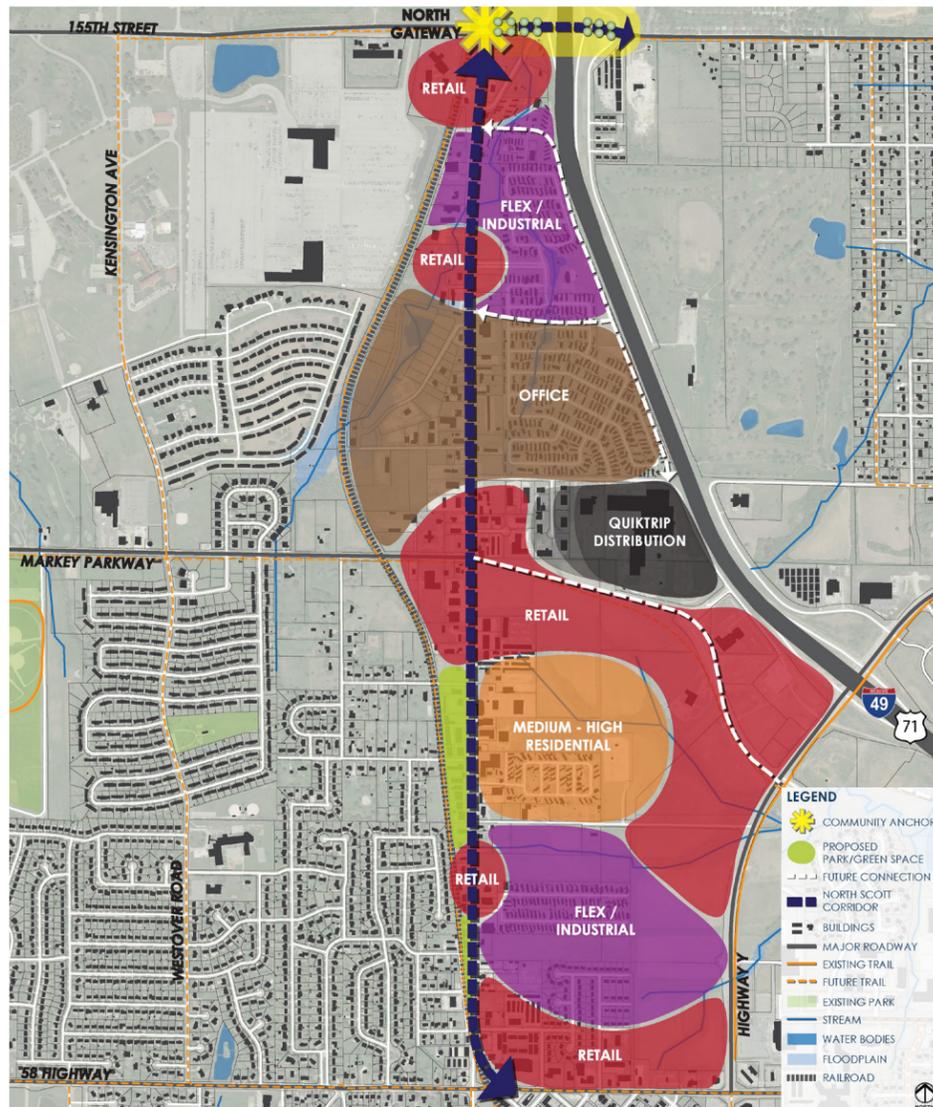


FIGURE 3.2 MIXED USE CORRIDOR CONCEPT

FIGURE 3.3 ADVISORY COMMITTEE REVIEW AND INVOLVEMENT

INITIAL STREET CONFIGURATION CONCEPTS

To complement the proposed land use concepts, the planning team developed three different street configuration concepts for the Advisory Committee to evaluate. The planning team also considered four lane and three lane options in a variety of configurations to stimulate discussion.

There were three different lane treatment options: a four lane continuous throughout the entire North Scott Corridor; a three lane continuous throughout the entire North Scott Corridor; and a combination of both four and three lane options for the North Scott Corridor with Markey Parkway being the separator (options shown in Figure 3.4). Each Advisory Committee member was given one vote to select which option they preferred.

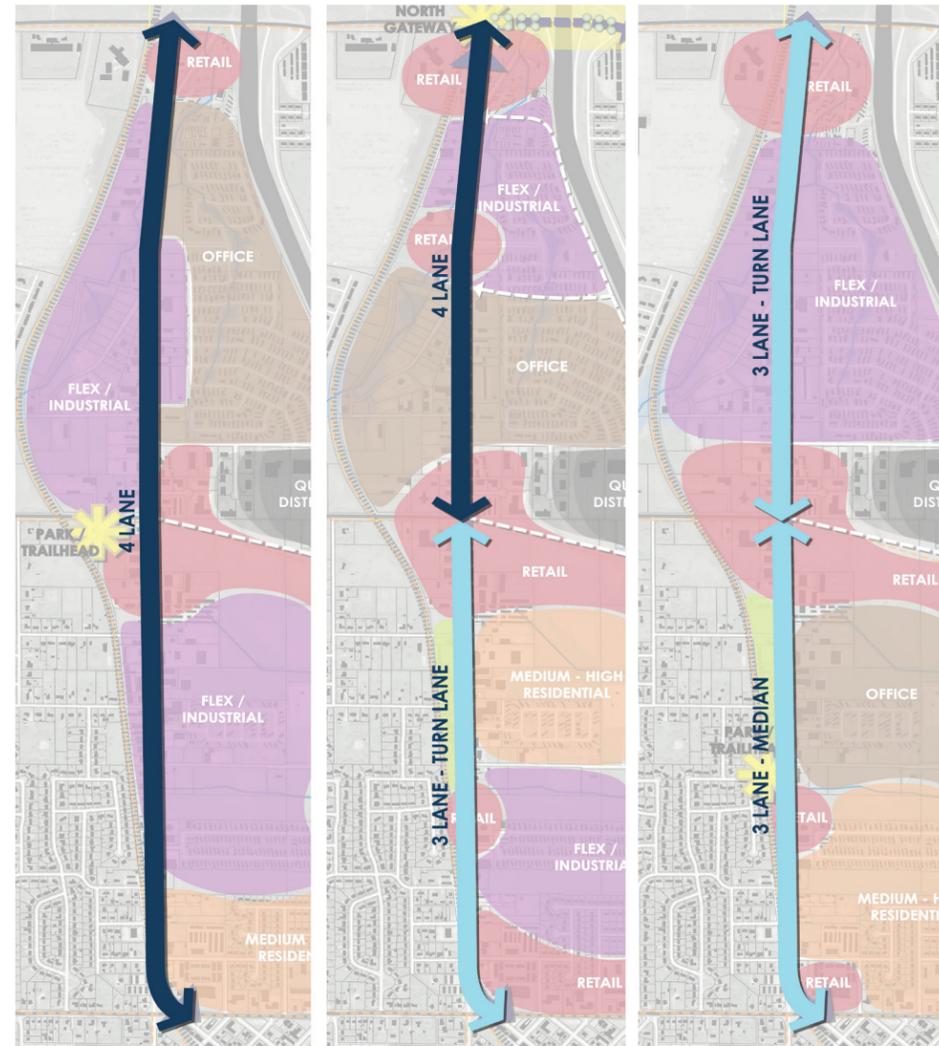


FIGURE 3.4 LANE TREATMENT OPTIONS

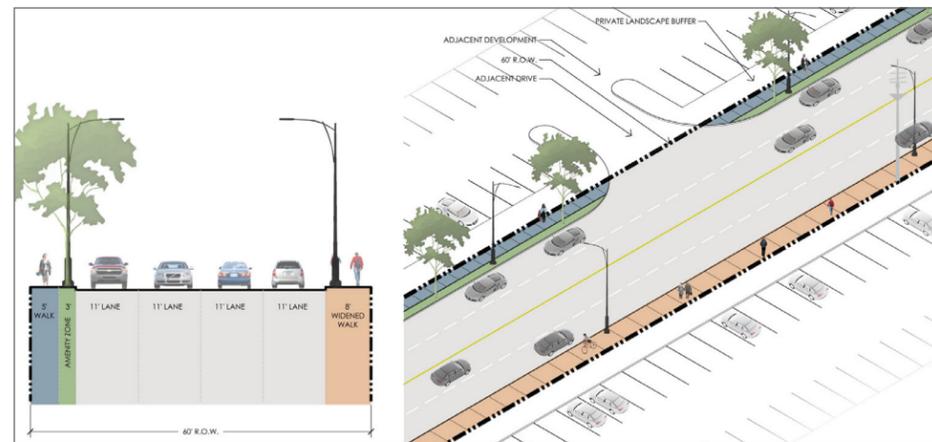
4 LANE OPTIONS

The planning team presented the committee with seven different lane configurations. This was a way to illustrate various options for utilizing the existing sixty foot right of way as both four lane and three lane options.

There were three (3) four lane options provided for the committee to choose from (shown on this page): one with a widened, multi-use widened sidewalk and 11' lanes; one with a continuous green buffer and 11' lanes; and one with 12' lanes and no green space.

There were four (4) three lane options provided for the committee to choose from (shown on the next page). Out of all of the three lane options, only one provided an option for a median in areas where a turn lane may not be needed. All of them did provide some type of consistency or rhythm with a green space or buffer between vehicular traffic and pedestrian sidewalks and all were shown with 12' wide lanes.

A summary of each of these four lane and three lane options is provided as follows:

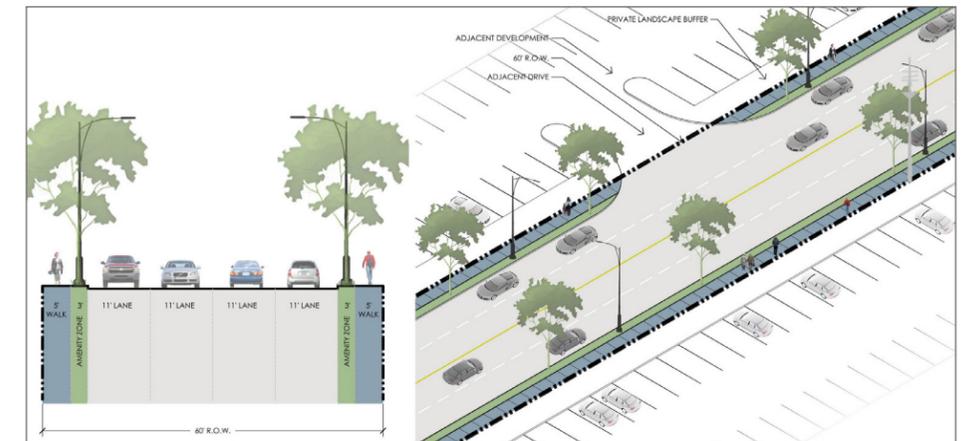


LANE CONFIGURATION: 11' LANES AND WIDENED WALK

This four lane configuration shows a widened walk component on one side of the road. This allows multiple uses on one side of the road, while the other side provides a small green amenity zone and a five foot walk. The lanes in this configuration are set at 11 feet wide.

LANE CONFIGURATION: 11' LANES AND GREEN BUFFER

This four lane configuration shows a consistent treatment along both sides of the road, providing a three foot amenity zone and a five foot walk. In order to achieve this green buffer on both sides of the roads, the lanes widths must be reduced to 11 feet.



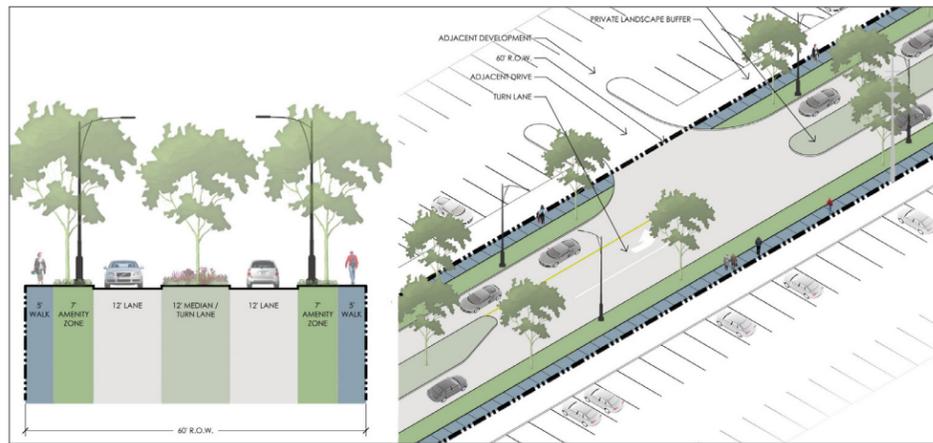
LANE CONFIGURATION: 12' LANES

This four lane configuration is most similar to the existing four lane segment currently along North Scott. The lanes would be 12 feet wide, while the remainder of space is dedicated to a six foot walk on either side of the road. No green space is attainable in this lane configuration option.

# ← 3 LANE OPTIONS →

## LANE CONFIGURATION: MEDIAN

This three lane configuration option has the most area for landscape. Not only is there a seven foot amenity zone that separates a five foot walk from vehicular traffic on both sides of the street, but a median would happen in areas where a turn lane is not needed. All lanes are set at 12 feet wide.

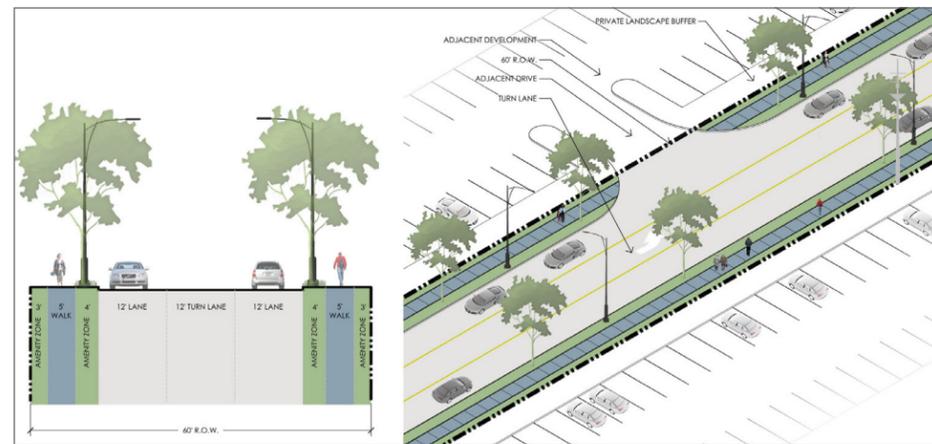


## LANE CONFIGURATION: GREEN BUFFER

This three lane configuration option is exactly the same layout as the street configuration show above except it does not include a median and instead includes a continuous turn lane. All lanes are set at 12 feet wide.

## LANE CONFIGURATION: TRAIL

This three lane configuration option incorporates a ten foot trail and a two foot amenity zone along one side of the road. The lanes are set at 12 feet wide and includes a center turn lane. A seven foot amenity zone and five foot walk is on the other side of the roadway.



## LANE CONFIGURATION: DIVIDED GREEN

The three lane configuration option has equal amounts of amenity zone and sidewalk available on both sides of the street. A four foot amenity zone separates cars from pedestrians, while a three foot amenity zone separates pedestrian from the adjacent development. The road has three 12 foot lanes with the center acting as a continuous turn lane.

## STREET CONFIGURATION ANALYSIS + PREFERENCES

The preferred street configuration option is similar to what exists on North Scott today: the 4/3 lane split. The Advisory Committee did not feel all four lane or all three lane options for the corridor would be appropriate. Figure 3.5 illustrates Advisory Committee members providing input on both lane and street configuration options.

The committee's overall preference for a 4 lane street configuration was the 12' lane option. The main issues expressed with this option were that there was not an opportunity to include green space. As for the 3 lane option, the committee preferred the divided green option; however, the committee felt the median option could also be considered, so long it does not prohibit providing effective access to adjacent land uses.

While there was discussion for incorporating bicycles into these options, the Advisory Committee felt the rail corridor should be utilized for this purpose if possible, and a parallel system within the limited 60' width right of way would not be necessary.



FIGURE 3.5 ADVISORY COMMITTEE EVALUATION

# 4 RECOMMENDATIONS

## LAND USE RECOMMENDATION

The community and Advisory Committee participants repeatedly expressed their vision for the North Scott Corridor as an area that should integrate industrial based businesses to provide good paying jobs and opportunities for commercial retail that compliments new development of the 163rd Street interchange. The design direction splits the corridor in two sections (north and south) with a center point at Markey Parkway. This approach allows two distinct land uses to co-exist and thrive, with a retail oriented focus south of Markey Parkway, and a flex/industrial focus to the north. The future plans for extending Markey Parkway creates a natural change in land uses at this intersection. The proposed land uses speak to the community's desire for a diverse corridor that will serve and support the surrounding area of the Belton community.

The proposed flex/industrial land use north of Markey Parkway was driven by the interstate access and visibility, and the existing four lane infrastructure from 155th Street to Markey Parkway. The established QuikTrip campus serves as the southernmost anchor for the flex/industrial land use. A larger retail node is illustrated at the intersection of 155th Street and North Scott. Development examples within this area could include a large, multi-use convenience store, restaurant options, as well as a potential hotel.

Retail land uses are shown consistently along the Markey Parkway extension, Y Highway/163rd Street, and 58 Highway. This will add to the development of the Belton Gateway, and other recent retail development along the east side of Y Highway/163rd Street. The retail land use expands along Highway 58, providing a connective retail edge. As illustrated by data results from participants in this study, some existing businesses along the North Scott Corridor play an important role in serving the community. Smaller

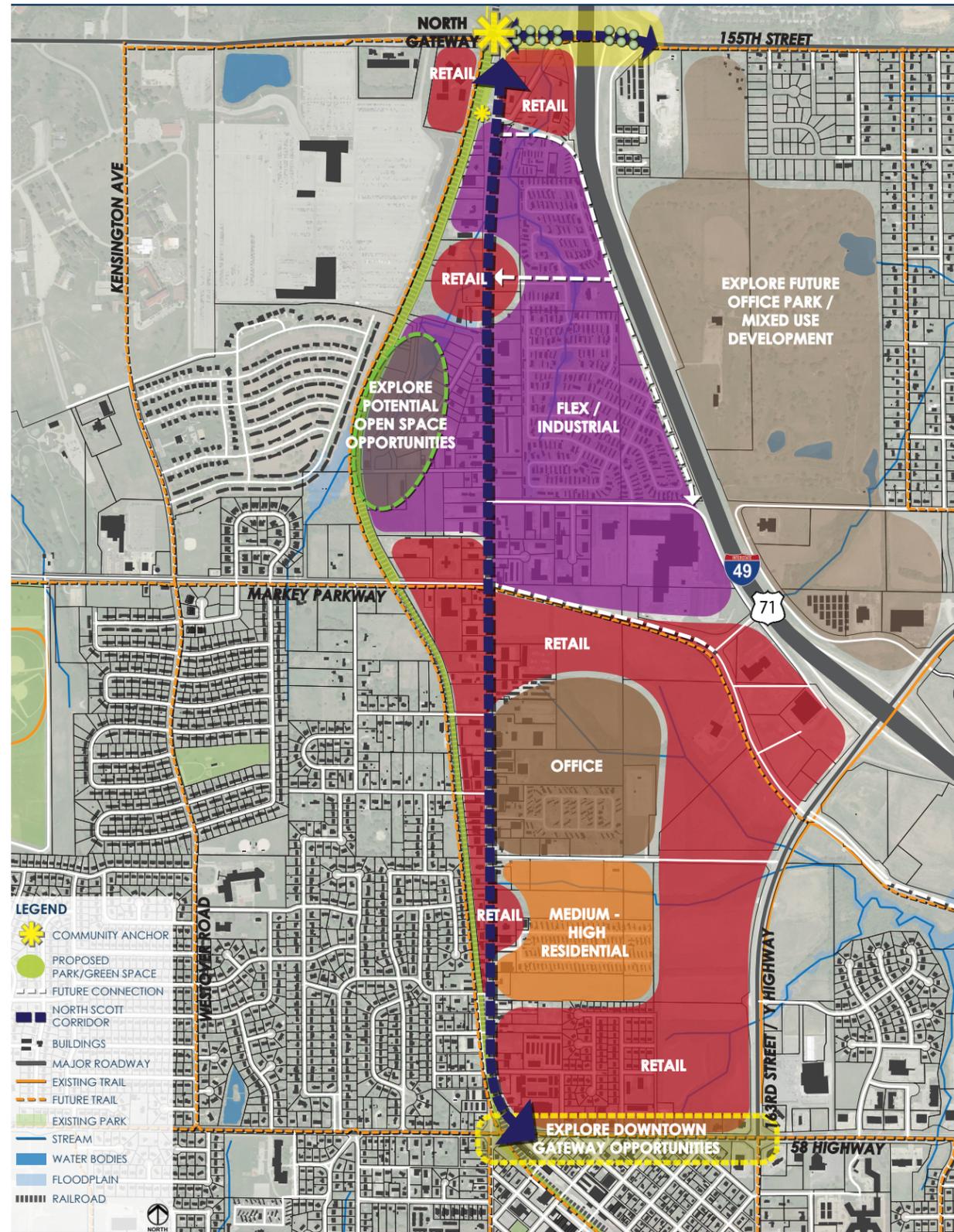


FIGURE 4.0 LAND USE RECOMMENDATION + PREFERRED CHARACTER IMAGES



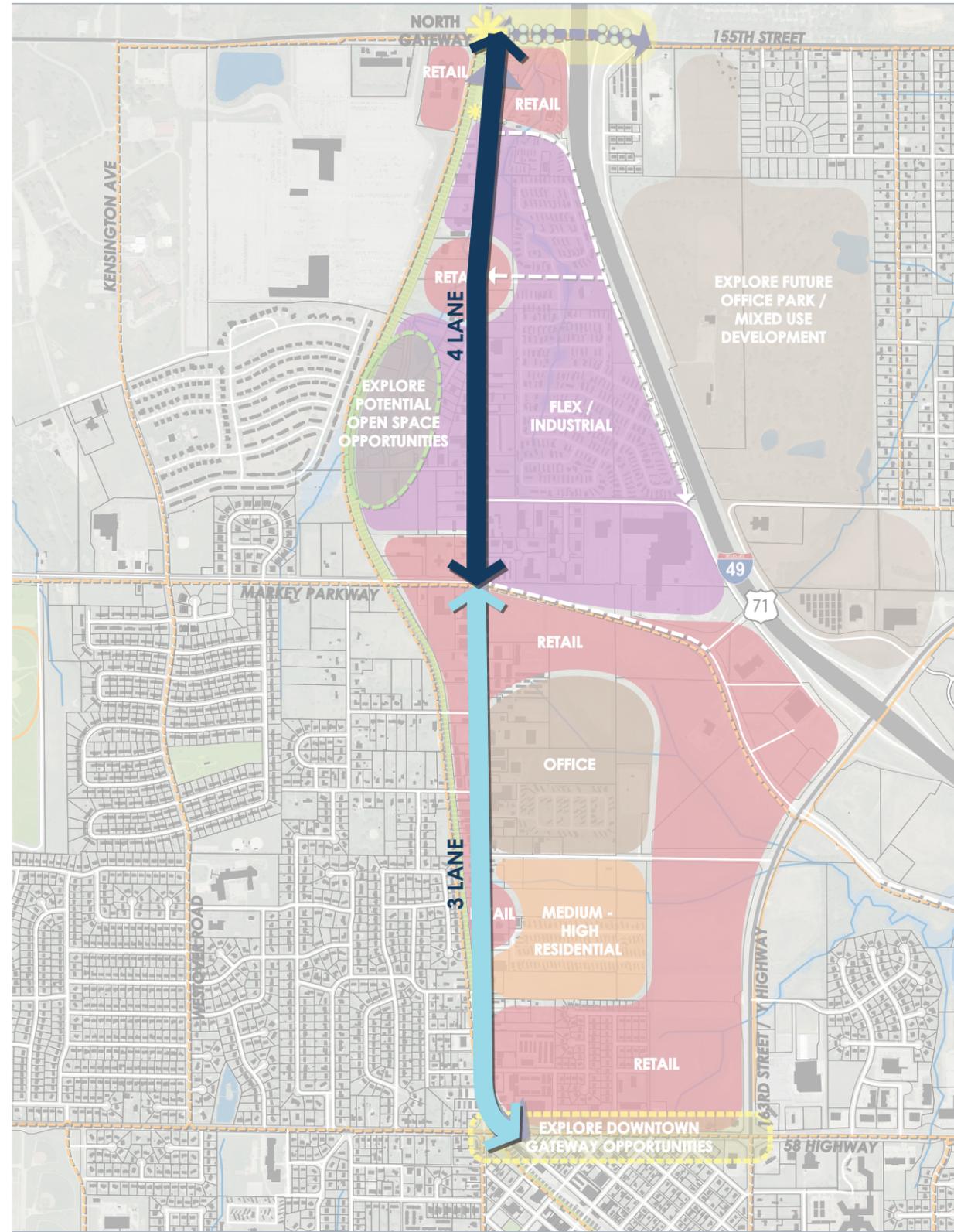


FIGURE 4.1 LANE TREATMENT RECOMMENDATION + PREFERRED CHARACTER IMAGES

retail nodes along the North Scott Corridor are shown in key locations on the land use plan for this primary reason.

These retail uses are anticipated to be supported by adjacent office and residential uses throughout the study area and beyond. Community elements, such as a north gateway and a community anchor near the proposed open space in the southern half of the corridor, illustrate the community's desire to enhance the visual character and community use of the corridor. The recommended land use plan and preferred development type examples are shown in Figure 4.0.

**STREET CONFIGURATION RECOMMENDATION**

The roadway configuration compliments the preceding land use recommendation and is a direct reflection of the Advisory Committee's suggestions (shown in Figure 4.1). The roadway configuration is recommended to stay the same as it is today in terms of number of lanes, both north and south of Markey Parkway. North of Markey Parkway will stay four lanes; while south of Markey Parkway will remain three lanes. The four lanes north of Markey Parkway will accommodate the anticipated truck traffic exiting from 155th Street and the Interstate-49 interchange traveling to proposed flex/industrial businesses. An additional frontage road, shown as a white dashed line along the west side of Interstate-49, will help provide access to businesses in this area.

The configuration for the south half of the corridor will maintain three lanes with one southbound lane, one northbound lane, and a center turn lane. This responds to proposed land uses in this area, including less land area served, due to the proximity of the rail on the west side. Community open space located along the west portion of North Scott, will emphasize a more open feeling for the corridor and will add green space as an extension of the public realm in this area.

**LANE CONFIGURATION RECOMMENDATION**

The configuration of the three and four lane options were further refined based on the input and suggestions provided by the Advisory Committee. Some additional options for acquiring additional right of way along the corridor was also considered as a result. These are summarized as follows:

**4 LANE OPTIONS**

The Advisory Committee's preferred concept of four (4) twelve foot lanes (illustrated at the top of Figure 4.2) is anticipated for use along the norther half of the corridor. However, there was much discussion focused on the road for additional green space within the street right of way.

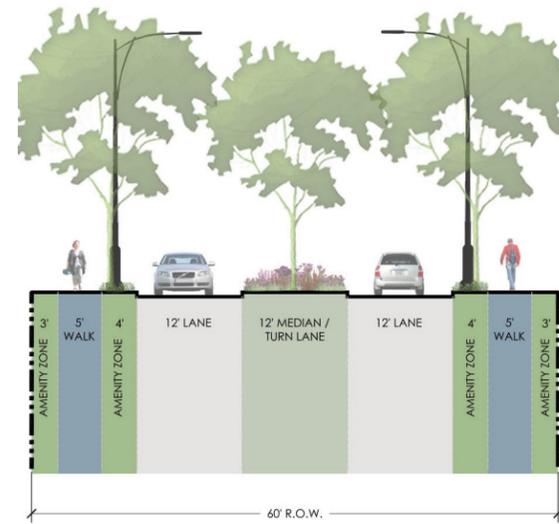
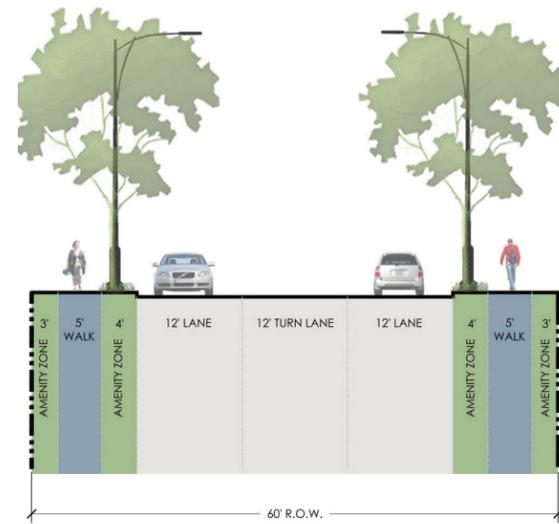
The drawback with utilizing the original option is the lack of any amenity zone to separate pedestrian traffic from vehicular traffic. There simply is not enough right of way to accommodate both the desired lane width as well as the requested amenity zone. The planning team responded with another lane configuration concept to incorporate the sought after amenity zone.

Shown at the bottom of Figure 4.2, this 4 lane configuration option supplies both desires expressed by the Advisory Committee: 12' lanes and green amenity zones to separate vehicular and pedestrian traffic. However, this will require acquiring a total of ten feet of additional right of way - five feet on each side of the corridor.



**FIGURE 4.2** 4 LANE CONFIGURATION RECOMMENDATION

3 LANE OPTION



3 LANE OPTIONS

The Advisory Committee preferred the 'divided green' three lane configuration option. This lane configuration, shown at the top of Figure 4.3, shows two directional lanes with a center turn lane. A four foot amenity zone separates pedestrian movement from vehicular traffic, while a three foot amenity zone separates the adjacent development from pedestrian movement. This allows the City of Belton to have the option of buffering the adjacent development with landscape, etc, within the right of way. The community strongly agreed that many of North Scott's businesses were not well-maintained, and that having the ability to implement a buffer between public and private property will help provide a more visually appealing streetscape appearance.

The three lane configuration does provide ample amount of amenity zones on each side of the road within the right of way. Amenity zones could potentially accommodate items such as street trees, landscape beds, turf, roadway lighting, pedestrian lighting, site furnishings and future bus stop locations. Exploration of the specific design and streetscape elements will need to be further studied as part of the recommended streetscape enhancements.

The 'divided green' option could potentially add green space to the overall configuration. As shown at the bottom of Figure 4.3, the street lanes and amenity zones could maintain the same widths but could also include a streetscape median in lieu of the turn lane at accepted locations. The Advisory Committee preferred the idea of an additional median, but was concerned it could create traffic flow issues when vehicles need to access businesses. It was agreed that the median should only be implemented in areas where it will not restrict access to existing businesses. Further study of the access drives in relation to median placement needs to occur as part of the recommended streetscape enhancement.

FIGURE 4.3 3 LANE CONFIGURATION RECOMMENDATION

# 5 NEXT STEPS

## PROVIDE STRATEGIC ECONOMIC DEVELOPMENT TOOLS

### IMPLEMENT INCENTIVE PROGRAMS TO ATTRACT NEW DEVELOPMENT

The City should explore opportunities for a targeted series of economic development tools to attract new high-quality development to invest along the North Scott Corridor. An Enhanced Enterprise Zone (shown in Figure 5.1) exists along the east side of the corridor. This was provided to “create sustainable jobs in a targeted industry and demonstrate impact on local industry cluster development” (<http://www.ded.mo.gov/>). Opportunities to expand the use of this incentive program and to identify other companion and/or alternate incentives that can assist in attracting jobs and investment along the corridor should be further studied and implemented. The use of Community Improvement Districts (CID), Transportation Development Districts (TDD), and other similar funding mechanisms to assist in improving public infrastructure in the area should also be explored. This could also be considered as part of the recommended Comprehensive Plan Update effort.

### PROVIDE INCENTIVE PROGRAMS TO REVITALIZE EXISTING PROPERTIES

The North Scott Corridor has a severe amount of diminishing property and businesses. Data collected during the public visioning and online survey concerning the community's thoughts of the North Scott Corridor further validates this statement. The City of Belton would benefit from a program that offers commercial business and/or property owners a grant for site or building improvements (see Figure 5.2 for a revitalization example). Incentives to the program could include options to receive more funding if the applicant chooses a local contractor, provide additional improvements on top of required improvements. There could also be a requirement that applicants must qualify as a licensed commercial business or property owner that generates sales, use and/or property tax revenues.

To further this effort, an overlay zoning district (i.e. “The North Scott Renewal District”) could be fabricated to better identify the businesses or properties that are eligible for potential funding. Not only would this clean up unattractive businesses that don't want to leave North Scott, but it would also maintain existing business that are essential to the history and establishment of the North Scott Corridor.

Another incentive program to encourage revitalization of existing properties could be an Enhanced Sales Tax Incentive Program. This program would give rebates of retail sales tax to business or property owners as a result of any capital improvements. This would provide motivation to existing business and property owners if they so choose to improve items within the existing right of way.

## EXTEND MARKEY PARKWAY

The City should continue to move forward towards improving and connecting the extension of Markey Parkway from the 163rd Street interchange northwest towards the North Scott Corridor. This important connection has an opportunity to be a catalyst in sparking growth in the central portion of the North Scott Corridor. Travel demand patterns in the area are anticipated to shift as a result of this new connection. The increased visibility and access to properties adjacent to this new street alignment will also strengthen commercial and industrial opportunities surrounding the new intersection with North Scott.

## UPDATE BELTON'S COMPREHENSIVE PLAN

While this initial corridor study outlines some recommended changes to the land use plan within the North Scott Corridor study area, there was specific discussion during the Advisory Committee meetings about the potential for a larger office/mixed-use development opportunity within the City of Belton. The former golf course area directly across Interstate-49 between 155th Street and 162nd Street was identified as a good candidate site, and was incorporated as an initial idea as part of the North Scott Corridor study. This was done recognizing this site is outside the boundary of the study area, and will need further exploration with property owners and neighbors prior to finalizing any land use recommendations for that area.

The City should strongly consider updating the Belton's Comprehensive Plan to ensure these and other opportunities are further explored and integrated into the community's strategy for future growth and development. Significant public input should be utilized in this type of planning process to ensure relevant issues are identified, explored, and addressed as part of the final plan recommendations.

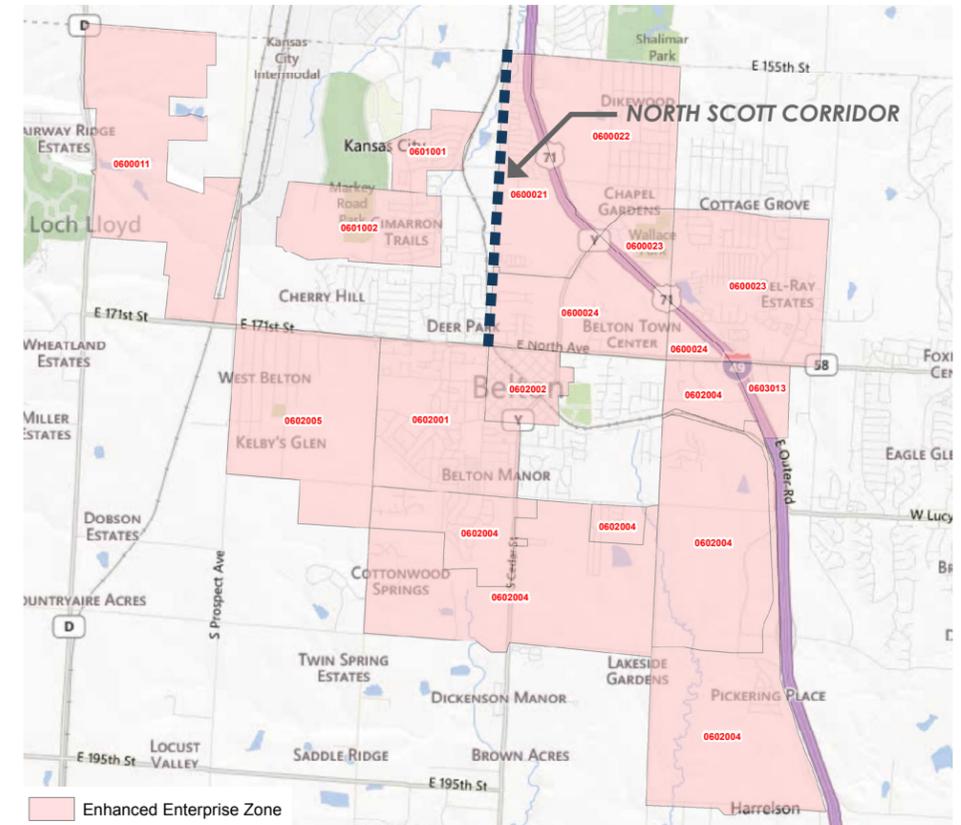


FIGURE 5.1 BELTON'S ENHANCED ENTERPRISE ZONES



FIGURE 5.2 REVITALIZATION EXAMPLES



**IMPROVE BELTON'S NORTH GATEWAY**

The City should plan for aesthetic enhancements to the 155th Street and Interstate-49 interchange, including extending these improvements west along the 155th Street corridor leading to the North Scott Corridor interchange. This entire area serves as the northern gateway into Belton, and should be treated as a major arrival moment. There is also some potential to explore a train depot/park structure as a destination facility as part of this northern gateway enhancement, which would require a parcel of land sufficient to provide parking and room for appropriate park amenities and features to create a welcoming sense of arrival into Belton.

A unique identity for the City should be created through a combination of signage, monuments, aesthetic grading, lighting, and landscape plantings. This design should complement the existing stone identity markers located near the Interstate-49 interchange areas. The final design of this gateway enhancement should also explore an archway feature over the North Scott Corridor, as it was a preferred alternative identified by those participating in the image preference exercise at the initial public meeting. Depending upon timing and funding availability, the City should also explore opportunities to incorporate a portion of these landscape planting improvements into MoDOT's construction plans for the Interstate-49 interchange replacement.

**CREATE A STREETScape MASTER PLAN FOR THE CORRIDOR**

There is an inherent need to improve the physical appearance along the entire North Scott Corridor, and the provision of streetscape design and aesthetic enhancement of the street will assist in establishing an expectation of quality throughout the corridor. A portion of the planning process has already established initial community streetscape preferences (shown in Figure 5.5). The City of Belton should proceed with an overall streetscape design master plan for the entire corridor that further explores the specific application of three and four lane street configurations and alternatives as outlined in this study.

In addition to modifying the physical layout of the street, these improvements are anticipated to:

1. Increase opportunities for green space/landscape plantings along the street.
2. Provide for defined driveway locations to make access and traffic flow more efficient throughout the corridor.
3. Incorporate a unique "look and feel" for lighting, street trees, site furnishings, and identification signage to promote this corridor as a preferred address in Belton and the surrounding area.



FIGURE 5.3 GATEWAY EXAMPLES

FIGURE 5.4 STREETScape COMPONENT EXAMPLE

FIGURE 5.5 PREFERENCE IMAGES FROM PUBLIC VISIONING WORKSHOP

**UPDATE THE CITY'S PARKS AND RECREATION MASTER PLAN**

As the City of Belton continues to grow and develop, including implementing the land use recommendations outlined in this corridor study, the need for adequate parks and recreation facilities should also be identified and addressed. The City should consider updating the future parks and recreation master plan in order to create an overall strategy for facility placement and connectivity throughout the entire community. This effort could stand alone as a separate study, or could also be considered as part of the recommended Comprehensive Plan update.

**EXPLORE RAIL + TRAIL OPPORTUNITIES**

The relatively poor condition of the existing railway does not make train operation feasible currently. Unless or until the track infrastructure is brought up to current railway conditions by the track's owner, the City of Belton should explore public-private partnership opportunities with the owner to utilize the track alignment (or a portion thereof) for use as a multi-use trail. There has been an identified need for maintenance to several track/street crossings. Unless something is done to address the deterioration of the overall condition of the railway in the near term, this situation will continue to decline. Additional analysis may be necessary to determine the financial impact these track crossing maintenance activities are imposing on the City's budget for existing street infrastructure, and other remedies may need to be explored.

Unless or until a viable commercial use for track reconstruction is identified by the owner, it is unlikely a funding mechanism will be identified to finance the wholesale track reconstruction and maintenance activities needed to support returning these tracks to productive and safe operation. As such, it is recommended that the City and the track owner begin discussions for creating a mutually beneficial partnership that would allow the track alignment to be utilized for a public use of some kind, which could include a multi-use trail to improve connectivity throughout the study area and to areas beyond. Additional analysis would be needed to explore whether there is ample room within the existing track right-of-way to allow for a parallel trail alignment of sufficient distance from a future track reconstruction, such that both uses could safely and effectively co-exist.

There may also be opportunities to explore utilizing the track infrastructure as a unique tourism opportunity. Partnership opportunities could be explored for creating a small scale, low-speed themed train ride experience. A new depot structure could serve dual use as a thematic park amenity along the North Scott Corridor and/or near Downtown Belton, and could become a unique area attraction for various holidays and special community events. Examples of successful commercial ventures incorporating a "Polar Express" trip for families during the winter holiday season can be found throughout the country. Creative adaptation of this infrastructure to provide a similar attraction could also be further explored, which might include public/private partnerships in order for it to be successful. Using the tracks at a much lower speed could also decrease the width of land needed to provide a safe distance for a parallel trail alignment to be implemented.

**DEVELOPMENT DESIGN GUIDELINES**

The City of Belton is experiencing more development demand than ever before, and this planning effort represents an opportunity to guide high-quality compatible growth along the North Scott Corridor. In order to stay ahead of the development curve, the City should consider establishing development design guidelines crafted specifically for use in this corridor. These guidelines should positively influence the way builders, developers, and property owners shape their development projects and physical improvements to their properties. They can also be used to proactively position the City of Belton to firmly establish and maintain a cohesive image and level of quality throughout the North Scott Corridor.

Care should be exercised to establish an appropriate level of quality and development character for the area, and should be targeted to the range of land uses anticipated and prescribed in this corridor study recommendations. These guidelines should establish expectations that are reasonable and achievable given anticipated market conditions and anticipated development densities. The community has already started this process by selecting some initial preferred images to guide this future effort (see Figure 5.8 for a selection of the community's preferred development images). The planning process to prepare and refine these guidelines should build on this initial effort to properly guide future corridor growth and revitalization.



**FIGURE 5.6** COMMUNITY SPACE EXAMPLE



**FIGURE 5.7** RAILS + TRAILS EXAMPLE



**FIGURE 5.8** PREFERENCE IMAGES FROM PUBLIC VISIONING WORKSHOP